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PROJ. #: 23-80000143

Received: 10/10/23 Paid: 10/10/23

PRE-APPLICATION

INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED

APPLICATION FEE

☒ PRE-APPLICATION \$50.00

PROJECT

PROJECT NAME: FDOT District 5 Central Corridor Truck Parking Facility - Seminole County Site
PARCEL ID #(S): 21-19-30-511-0A00-0000; 21-19-30-511-0000-0020; 21-19-30-511-0000-0010; 21-19-30-502-0400-0000;
21-19-30-511-0000-0030; 16-19-30-5AC-0000-026B; 16-19-30-5AC-0000-026E
TOTAL ACREAGE: 17.32 AC BCC DISTRICT: District 5
ZONING: PD & A-1 FUTURE LAND USE: HIPTI; High Intensity Planned Development
-Target Industry

APPLICANT

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CONSULTANT

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PROPOSED DEVELOPMENT (CHECK ALL THAT APPLY)

☐ SUBDIVISION ☐ LAND USE AMENDMENT ☒ REZONE ☒ SITE PLAN ☐ SPECIAL EXCEPTION

Description of proposed development: Construction of a State owned and maintained truck parking facility.

STAFF USE ONLY

COMMENTS DUE: 10/20	COM DOC DUE: 10/26	DRC MEETING: 11/1
<input type="checkbox"/> PROPERTY APPRAISER SHEET <input type="checkbox"/> PRIOR REVIEWS:		
ZONING: PD	FLU: HIPTI	LOCATION: on the west side of Monroe Rd, south of Orange Blvd
W/S: Seminole County	BCC: 5: Herr	

Agenda: 10/27

The intent of this project is to construct a Florida Department of Transportation owned and maintained truck parking facility to maximize the parking count on the selected site. The site is located in Seminole County between I-4 to the west and U.S. 17 (Monroe Road) to the east. The site is also bordered by Orange Boulevard to the north and School Street to the south.



D5 Truck Parking Facility Central Corridor

Seminole County, Florida

FPID 446445-1-32-01

Design Narrative and Documentation

Interim Print October 2023

Prepared By:

Bentley Group Inc.

651 West Warren Ave. Suite 200

Longwood, Florida 32750

Molly A. deVivero, P.E., State of Florida, Professional Engineer, License No. 64860

This item has been digitally signed and sealed by

on the date indicated here.

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

Design Documentation

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SECTION 1 Project Description and Summary of Project Requirements

A. Introduction

The intent of this project is to construct a Florida Department of Transportation owned and maintained truck parking facility to maximize the parking count on the selected site. The site is located in Seminole County between I-4 to the west and U.S. 17 (Monroe Road) to the east. The site is also bordered by Orange Boulevard to the north and School Street to the south.

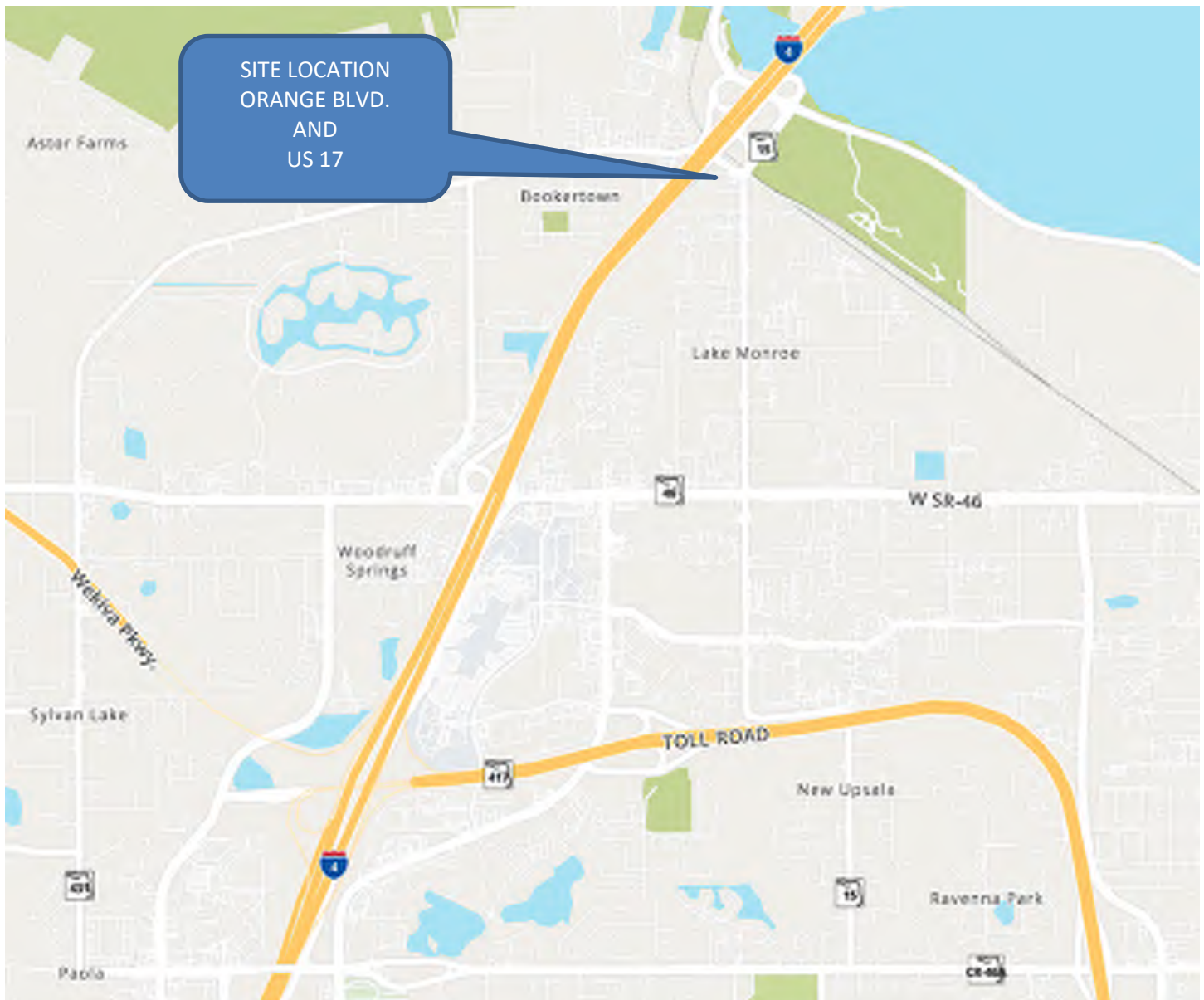


Figure 1: Project Location Map

Design Documentation

The selected site was evaluated in a Project Development and Environmental (PD&E) study for Truck and Freight Alternative Site Analysis (TFASA) for the I-4 corridor within Osceola, Orange, Seminole and Volusia counties. The PD&E study was completed under FPID 447724-1-22-01. The following documentation generated as part of the PD&E study was made available by the Department and used for the development of this design documentation.

- TFASA PD&E Study “Next Steps for Project Development” Memo dated 8/31/22: Refer to Section 1.C for project requirements and refer to Appendix E for a copy of this memorandum.
- TFASA PD&E Study Natural Resources Technical Memorandum (NRTM) dated September 2022: Refer to Section 1.D for project impacts and Appendix E for a copy of this memorandum.
- TFASA PD&E Study Final Project Traffic Analysis Report (PTAR) dated September 2022: Refer to Section 1.E for project impacts and Appendix E for a copy of this memorandum.
- TFASA PD&E Study Contamination Screening Evaluation Report (CSER) dated August 2022: Refer to Section 1.F for project impacts and Appendix E for a copy of this memorandum.
- TFASA PD&E Study Conceptual Drainage Report FINAL dated August 2022: Refer to Section 1.G for project impacts and Appendix E for a copy of this memorandum.
- TFASA PD&E Study Comment & Coordination Report dated August 2022: Refer to Appendix E for a copy of this memorandum.
- TFASA PD&E Study Draft Preliminary Engineering Report dated July 2022: Refer to Appendix E for a copy of this memorandum.

The following subsection summarizes the recommendations of the PD&E documents received as of January 1, 2023, outline scoped facility requirements, document design criteria and provide an overview of facility programming. In the following subsections *items in red italic are outstanding elements, tasks specifically omitted from the scope of this project or design that will be developed for future submittals.*

B. Proposed Project Facilities

Scope Defined Facility Requirements

- Include accommodations for tandem trucks
- Include area for mobile truck repair vehicle
- Advanced signage
- Electrical charging (EV) station infrastructure
- Truck Parking Availability System TPAS implementation and CCTV monitoring
- Enclosed pet area with pet waste bag dispenser and waste receptacle
- Windshield wash area

Facility Design Manual Defined Facility Requirements

- Maintenance, vending and visitor parking
- Truck parking (design vehicle and accommodations for tandem trucks)
- Security staff parking
- Facility fencing
- Maintenance building
- Dumpster enclosure
- Site lighting
- Pavilions
- Waste Receptacles

Design Documentation

PD&E Defined Facility Requirements

- Designated and signed as truck parking facility
- No limit on truck parking durations
- Landscape and pond buffers

Scope excluded items and justification

- Air compressor (most truck drivers carry portable tire compressors; this function can also be provided by a mobile truck repair vehicle)

C. Specific PD&E Commitments for the Seminole County Site

The following items are commitments made and implementation measures identified in the “Next Steps for Project Development” PD&E memorandum dated 8/31/2022.

- Provide a landscape buffer (size not specified) along Orange Boulevard to provide visual buffer. No specific buffer requirement minimum widths were noted within the PD&E documentation. Therefore, Bentley will proceed using a buffer width of 25 feet will be allotted. This width is consistent with the minimum buffer width required along SR 46, reference Seminole County Land Development Code (SCLDC) Sec. 30.1205. The 25’ buffer will contain a berm along Orange Boulevard.
- A commitment was made to Seminole County to provide a large buffer along US 17/92 with extensive plantings and a pond.
- Provide a tight radius return and raised island on School Street to prevent trucks from exiting and traveling west
- Provide signage for trucks exiting the site to travel east on School Street
- Provide signage for trucks to turn right at the eastbound approach to US 17/92 at School Street for SR 46 access to I-4
- A signalized entrance at School Street is proposed for the I-4 Beyond the Ultimate Segment 3 (BtU3) project as recommended in the PD&E Final PTAR
- Coordination with the I-4 BtU3 team for the realignment of Orange Boulevard and the pond storage capacity defined herein

D. PD&E Environmental Recommendations

The PD&E natural resources technical memorandum was prepared to assess the project site and identify potential impacts to natural resources within the site including impacts to habitat for protected species, wetlands and surface waters. The conclusion of the memorandum is that the proposed project will not jeopardize the continued existence of protected species and/or will not result in the destruction or adverse modification of critical habitat. The memorandum noted that the project will directly impact approximately 1.4 acres of wetlands. The following table from the noted memorandum identifies the species that were evaluated and project effect determinations.

Common Name	Scientific Name	Status	Effect Determination
Florida scrub jay	<i>Aphelocoma coerulescens</i>	FT	NO EFFECT
Burrowing owl	<i>Athene cunicularia floridana</i>	ST	NAEA
Little blue heron	<i>Egretta caerulea</i>	ST	NAEA
Tricolored heron	<i>Egretta tricolor</i>	ST	NAEA
Southeastern American kestrel	<i>Falco sparverius Paulus</i>	ST	NEA
Florida sandhill crane	<i>Grus canadensis pratensis</i>	ST	NAEA
Bald eagle	<i>Haliaeetus leucocephalus</i>	BGEMA/ MBTA	--
Eastern black rail	<i>Laterallus jamaicensis jamaicensis</i>	FT	NO EFFECT
Wood stork	<i>Mycteria americana</i>	FE	MANLAA
Roseate spoonbill	<i>Platalea ajaja</i>	ST	NAEA
Audubon's crested caracara	<i>Polyborus plancus audubonii</i>	FT	NO EFFECT
Everglade snail kite	<i>Rostrhamus sociabilis plumbeus</i>	FE	NO EFFECT
Southern fox squirrel	<i>Sciurus niger niger</i>	M	--
West Indian manatee	<i>Trichechus manatus</i>	FT	NO EFFECT
Florida black bear	<i>Ursus americanus floridanus</i>	M	--
Eastern indigo snake	<i>Drymarchon corais couperi</i>	FT	MANLAA
Gopher tortoise	<i>Gopherus polyphemus</i>	C/ST	MANLAA
Florida pine snake	<i>Pituophis melanoleucus mugitis</i>	ST	NEA
Many-flowered grass-pink	<i>Calopogon multiflorus</i>	ST	NEA
Chapman's sedge	<i>Carex chapmannii</i>	ST	NAEA
Sand butterfly pea	<i>Centrosema arenicola</i>	SE	NO EFFECT
Pygmy Fringe-tree	<i>Chionanthus pygmaeus</i>	FE	NO EFFECT
Okeechobee gourd	<i>Cucurbita okeechobeensis</i>	FE	NO EFFECT
Rugel's pawpaw	<i>Deeringothamnus rugelii</i>	FE	NO EFFECT
Hartwrightia	<i>Hartwrightia floridana</i>	ST	NAEA
Star anise	<i>Illicium parviflorum</i>	SE	NAEA
Nodding pinweed	<i>Lechea cernua</i>	ST	NEA
Florida spiny-pod	<i>Matelea floridana</i>	SE	NEA
Celestial lily	<i>Nemastylis floridana</i>	SE	NAEA
Florida beargrass	<i>Nolina atopocarpa</i>	ST	NEA
Florida willow	<i>Salix floridana</i>	SE	NAEA
Clasping warea	<i>Werea amplexifolia</i>	FE	NO EFFECT

MANLAA = May Affect, Not Likely to Adversely Affect

NEA = No Effect Anticipated

NAEA = No Adverse Effect Anticipated

FE = Federally Endangered

FT = Federally Threatened

SE = State Endangered

SF = State Threatened

M = Managed

C = Candidate

Figure 2: Table 7: Proposed Effect Determinations for Protected Species (From the PD&E NRTM for Seminole County Site 1B)

The project design includes determination of final wetland impact total area. As coordinated with the St. Johns River Water Management District, historical onsite wetland were mitigated in a previous Environmental Resource Permit.

PD&E Traffic Recommendations

The following recommendations were made within the PD&E PTAR.

- Trucks leaving the site will be signed for a left turn only onto School Street toward US 17
- Trucks traveling eastbound on School Street will be signed for a right turn onto US 17 to return to I-4 via SR 46
- Signalization of the intersection of School Street and US 17 was not a recommendation of the PTAR but will be incorporated into the BtU3 project for the realignment of Orange Boulevard
- The design takes into consideration the future I-4 BtU3 project consisting of a full signalized intersection at School Street. A preliminary concept included in the PTAR is provided below. The plans include the line work provided by the Department for the Orange Boulevard realignment at School Street.



Figure 3: Conceptual Interchange Configuration (From the PD&E PTAR)

E. PD&E CSER Findings and Recommendations

The following finds of the CSER will affect the design or right of way clearing.

- Hydrologic Features
 - Not located within an OFW
 - Located within St. Johns River Water Management District (SJRWMD) WBID 2893C – St Johns River Above Wekiva River, not impaired for nutrients but is within BMAP and TMDL of the Middle St. Johns River
 - Hydraulic Soil Group A/D
- Environment Risk
 - Phase I will be completed by the PD&E team.

F. PD&E Recommended Stormwater Management Plan

The volumetric recommendations and analysis within the PD&E Conceptual Drainage Report Final (CDRF) are preliminary and the conceptual design pond configuration is based on preliminary seasonal high-water table (SHWT) and wetland hydraulic information. The PD&E report required treatment and attenuation volume (RT+AV) was estimated to be 6.16 ac-ft; the PD&E reports estimated conceptual design RT+AV was 6.32 ac-ft; the PD&E reports' estimated pond area was 4.01 acres.

The PD&E CDRF documents a needed attenuation volume of 1.43 ac-ft and a treatment volume of 0.56 ac-ft be included in the pond design in addition to that needed for the project area to be utilized by the future *I-4 Beyond the Ultimate* project.

G. PD&E Noted Services Deferred to the -32 Design Phase

The following tasks were identified to be completed in the -32 (design phase) scope of services in the "Next Steps for Project Development" PD&E memorandum dated 8/31/2022; or included in the scope of services as standard Environmental Clearances and Reevaluation effort.

- Completion of a Cultural Resources Assessment Survey (reference June 7, 2022, email from Catherin Owen to Megan Owens excluding CRAS design services) Note: the executed contract did keep the design team member SEARCH on the contract)
- Design year noise impacts will be completed by the PD&E team.
- Level I CSER will be completed by the PD&E team.
- Addition environmental analysis and verification of need including COA, SCE and noise will be completed by the PD&E team. Re-eval needed instead of COA.
- Species specific surveys and permitting
- PD&E team is reviewing the utilities to conduct the UAP.
- Level II CSER will be completed by the FDOT CAR contractor as applicable subsequent to the Level I CSER. This will be coordinated by FDOT PLEMO and the PD&E Study Team in coordination with the Design Team.

H. Design Criteria

- FDOT 2020 Facilities Design Manual
- FDOT Design Manual
- FDOT Drainage Manual
- FDOT Design Standards
- FDOT Florida Green Book
- FDOT Flexible Pavement Design Manual

Design Documentation

- FDOT Rigid Pavement Design Manual
- Florida Building Codes

I. Facility Design & Programming (Site/parking; building)

As a starting point for designing the Truck Parking restroom facilities, Bentley reviewed the 2020 Building Facilities Design Manual (BFDM). Since the BFDM does not currently contain standardized guidance for a Truck Parking Facility, Bentley considered the design requirements for rest areas and weigh stations. The following sections note similarities and key differences.

1. Similar to a typical rest area, the truck parking facility design is intended to serve a transitory user group.
2. The rest area facility is open for use 24/7/365; weigh station facilities may close intermittently.
3. Security is a predominant factor in the layout of both facilities.
4. Rest area, restroom fixture unit count and parking capacity are typically sized based on 20-year ADT projections. (Standardization of rest area facilities provided for small and large restroom building sizes based on ADT projections of 35,000 and 70,000 vehicles respectively). By contrast, weigh station parking capacity may be influenced by ADT projections; however, weigh stations are not subject to a specific BFDM requirement.

Although there are similarities to standard rest area and weigh station design, there are also key differences between the proposed truck parking facility and these standard facilities that must also be considered:

1. The intended user group for this facility will be limited to the professional trucking industry.
2. The truck parking facility use is transitory similar to a typical rest area, but the requirements are different. The expected duration of a user's stay at a rest area may be anywhere from 10 minutes to an hour; in general, long duration and extended overnight stays are discouraged. By contrast, federal law requires truck drivers to rest 10 hours for every 14-hour driving window; a truck parking facility must therefore accommodate extended stops. The need for extended stops is also apparent by current driver behavior at existing rest areas. FDOT will not limit the parking duration for truck parking facilities unlike rest area facilities.
3. The truck parking facility use is transitory similar to a typical weigh station. However, weigh stations/inspection stations are designed to accommodate a much lower parking capacity. The greater parking capacity of the proposed truck parking facility means the restroom facilities must be scaled larger to accommodate the use.
4. Security considerations will also need to scale upward based on the larger parking capacity.
5. This facility is focused on a specific user group as opposed to typical rest areas. Therefore, calculations utilizing ADT data is based on the specific trucking industry user group.
6. Based on truck only ADT calculations, the fixture count requirements for the facility will not coincide with standardized rest area fixture requirements contained in the BFDM.
7. Since the targeted users are expected to remain on site for extended periods, consideration must be given to providing amenities suitable to this user group.

To address the differences between a dedicated truck parking facility and a rest area with respect to fixture counts, Bentley prepared a technical memorandum to establish minimum fixture count requirements based on the 2020 Florida Plumbing Code. A subsequent team's meeting was conducted with Central Office, CO, to discuss the memorandum. CO team members determined that the approach defined in the memorandum was reasonable, consistent with truck weight station facilities, sound and defensible. Refer to Appendix A for a copy of meeting

Design Documentation

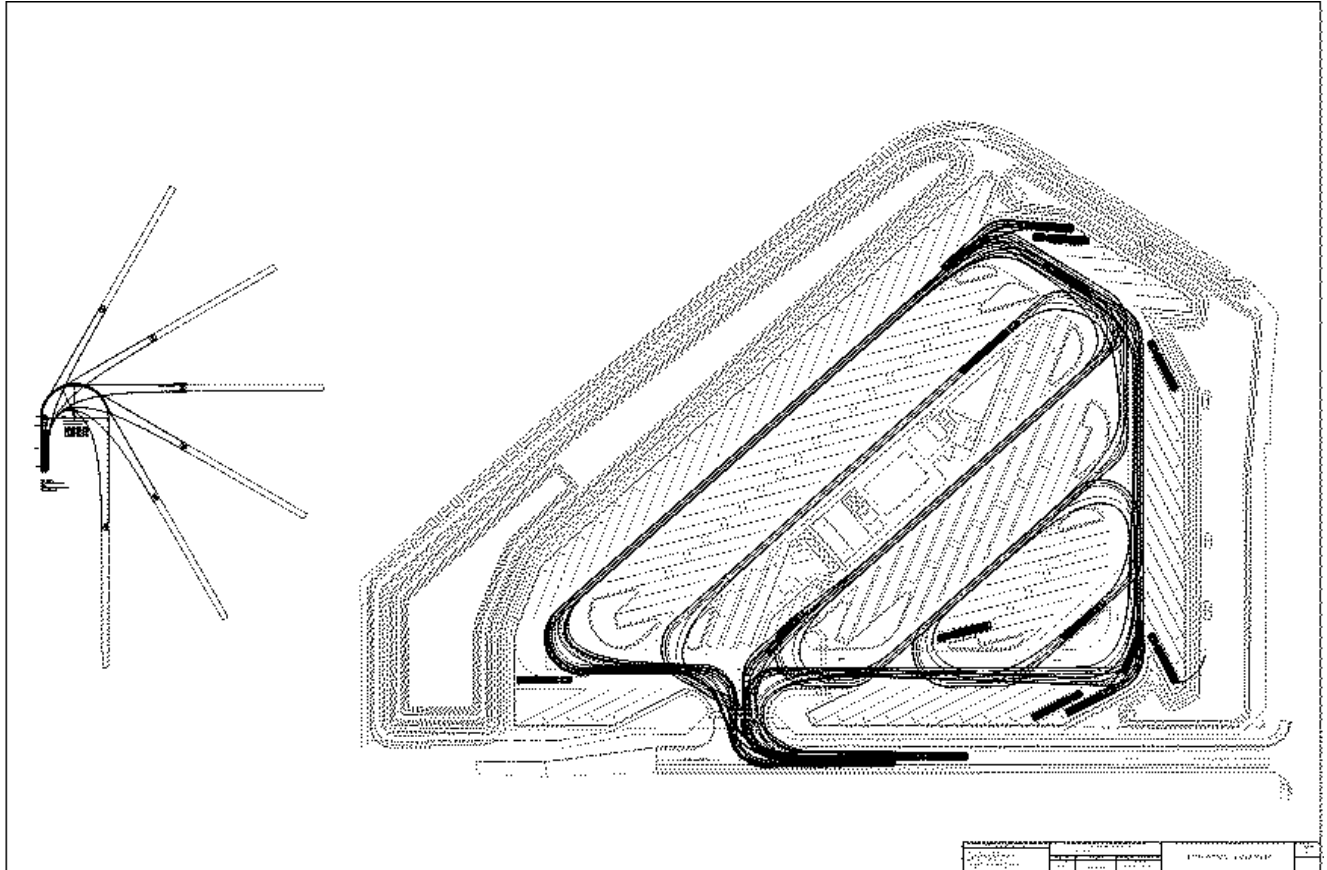
minutes, the technical memorandum and email coordination with CO. The minimum fixture count recommendations included in the memorandum are as follows based on total parking spaces provided or fraction thereof:

- Parking count less than 50 spaces:
 - 1 water closet, 1 lavatory, 1 water fountain and 1 service sink
- Parking count equal to 50 spaces and less than 150 spaces:
 - 1 water closet and 1 lavatory for every 25 spaces and 1 water fountain
 - 1 service sink for every 100 spaces
- Parking count equal to or greater than 150 spaces:
 - 1 water closet and 1 lavatory for every 25 spaces and 1 water fountain
 - 1 service sink for every 100 spaces
 - 1 unisex restroom for every 150 spaces

SECTION 2 Design Documentation and Project Criteria

A. AutoTurn Analysis

The autoturn analysis included in Appendix B utilizes the FDOT design vehicle Florida Interstate Semitrailer (WB-62FL). For oversized vehicle used in the autoturn analysis to demonstrate accommodating larger trucks is the WB-109D. The use of the WB—109D is not intended to imply that it is the design vehicle for this site.



B. Parking

The parking facility is designed to maximize the number of parking spaces by utilizing both pull through and back in parking. While back in parking is not the preference for truck parking facilities, due to the restricted right of way it is a viable solution. The parking angle is 27 degrees to better facilitate parking maneuvers. To aid in turning and parking maneuvers, one way drive aisle are 25 feet in width and two way drive aisles are 50 feet.

C. Building

1. Life Safety – Restroom Building

- The building adheres to the requirements of the 2020 editions of the Florida Building Code, the Florida Building Code, Accessibility, the Florida Building Code, Mechanical, and the Florida Building Code, Plumbing. Other codes also adhered to are the Florida Fire Prevention Code, 7th edition, and the 2017 edition of NFPA 70, National Electrical Code.
- The building is designed to be a Type IIB, unprotected, un-sprinklered building.

- The building is to be primarily a business occupancy, with an occupant load of 31. The remaining area of the building is storage occupancy, with an occupant load of 2. The total occupant load of 33 was utilized to determine the required exit widths.
- As mentioned previously, the plumbing fixture counts required were based on the number of parking spaces (1 fixture for every 25 parking spaces). This calculation accommodates the needs of the occupants at the facility, and far exceeds the requirements based under the Florida Building Code.

2. Life Safety – Restroom Building

- The building adheres to the requirements of the 2020 editions of the Florida Building Code, the Florida Building Code, Accessibility, the Florida Building Code, Mechanical, and the Florida Building Code, Plumbing. Other codes also adhered to are the Florida Fire Prevention Code, 7th edition, and the 2017 edition of NFPA 70, National Electrical Code.
 - The building is designed to be a Type IIB, unprotected, un-sprinklered building.
 - The building is to be primarily a storage occupancy, with an occupant load of 2. The total occupant load of 2 was utilized to determine the required exit widths.
3. Roof stormwater drainage calculations for all structures within the facility are based on 10.1" of rainfall per hour. Both the gutter and downspout sizes are derived from both the roof size and the hourly rainfall used.
 4. LEED Certification: The truck parking facility is to be commissioned per LEED v.4.1 SD+C: NC guidelines and requirements. The project goal is to achieve LEED Certified under the LEED Green Building Rating System. The structures that will not be a part of the LEED certification are the picnic pavilions and the maintenance building. The detailed information is illustrated under the Basis of Design (BOD) and the Owner's Project Requirements (OPR) documents that are under a separate cover.
 5. Refer to Appendix J for the detailed documentation of the building envelope requirements. The buildings are based off section C402 within the Florida Building Code, Energy Conservation, 7th Edition.

D. Drainage

1. Drainage calculations are on-going and will be provided under separate cover with the Phase II Submittal. The items highlighted in this section are the general key design requirements and do not intend to take the place of the drainage design narrative and report.
2. I-4 BTU Volume Commitment: Treatment volume = 0.56 ac-ft.; Attenuation volume = 1.43 ac-ft This requirement has been discussed with the BtU design team and coordination is ongoing with the BtU calculations to ensure the required project treatment and attenuation volumes are provided without including the same project area twice.
3. SJRWMD Criteria:

Water Quality: Due to the relatively high ground water table the stormwater attenuation and treatment will be accomplished utilizing wet detention ponds. The SJRWMD water quality standards for wet detention pond design are:

- One inch (1") of runoff over the drainage area or two and a half inches (2.5") of runoff from the impervious areas. The drainage area included in the design is the total site / property area plus any new roadway area or modified roadway area for access to the facility. Recovery of the water quality treatment volume is to be accomplished through an orifice with the invert set at the average wet seasonal water table elevation and sized to drawdown one-half of the treatment volume between 24 and 30 hours after the design storm event. No more than one-half of the treatment volume can be discharged within the first 24 hours.
- A pre versus post nutrient loading analysis is required as the project is within the Middle St. Johns River basin with a defined Best Management Action Plan and total maximum daily load requirements for nitrogen and phosphorus as noted above.

The wet pond, Pond 100, volumes will be augmented by a retention pond, Pond 200, in order to provide the total required treatment and attenuation volumes while providing pretreatment to meet nutrient loading requirements.

Water Quantity: As previously noted, the project is an open basin; therefore, the post-developed peak discharge, volume and rate, is to be equal to or less than the pre-developed peak discharge for the 25-year/24-hour and mean annual storm events using SJRWMD rainfall depths and the Natural resources Conservation Service (NRCS) Type II Florida Modified 24- hour rainfall distribution with an antecedent moisture condition II.

Wet Detention Pond Configuration Requirements:

- Length to width ratio of 2:1
- Permanent pool volume (PPV) sized to provide a 14-day residence time
- At least 30% of the wet pond surface area is to consist of a littoral zone with 1:6, vertical to horizontal, slope or flatter
Or
An additional 50% of the required PPV
Or
Dry pre-treatment equal to the treatment volume required for retention, underdrain, exfiltration or swale systems
- Maximum pond depth of 12 feet and a mean pond depth between 2 and 8 feet
- Side slopes are not to exceed 1:3, vertical to horizontal, measured from the control water (orifice) elevation to two feet below the control water elevation

The project as designed:

- Meets the SJRWMD wet detention pond volume criteria
- A littoral zone between elevations 7.50' and 5.50' will be provided for Pond 100

4. FDOT Criteria

Water Quality: As required by the SJRWMD

Water Quantity: For open basins subjected to historical flooding the post-development discharge shall not exceed the pre-development discharge for the critical duration storm (1-hour through 3-day) up to the 100-year storm. Otherwise as required by the SJRWMD.

Maintenance Berm: 20-foot minimum berm between the control elevation and the right of way line with at least 15 feet closest to the pond having a slope of 1:8 or flatter. Inside edge of maintenance berm shall have a minimum radius of 30 feet at the freeboard elevation.

Freeboard: One foot (1') elevation difference between the maximum design stage and the inside edge of maintenance berm.

Side Slopes: 1:4, vertical to horizontal slope or flatter above the control water elevation (or retention pond bottom) and 2' below the control water elevation.

Fencing: Install fences around ponds only when a documented maintenance need for restricted access has been demonstrated. The installation of fencing around stormwater ponds requires a Design Variation. Where approved, make sure fences are context sensitive and do not detract from the appearance of the ponds or adjoining property.

When requesting the approval of a Design Variation to install fence around stormwater management facilities, the conditions below, when properly documented, typically are acceptable justifications for ponds designed to be permanently wet (permanent design water depth of two feet or greater):

- Above-water pond slopes steeper than 1:4 are unavoidable. Note: Stormwater permits typically require wet ponds to be fenced when the above-water slopes of the pond are steeper than 1:4. Ponds that enjoy the benefit of fence at the right-of-way line need no additional fencing around them.
- A hidden hazard occurs within five feet of the water's edge.
- The site is likely to experience significant exposure to children or the elderly.

5. School Street Drainage

The proposed design includes travel lane widening, shoulders, and a sidewalk for truck maneuverability and public safety from the entrance of the truck parking facility east to the connection of US 17. Due to the existing elevations at the roadway connection points, southern boundary elevations of the right of way, existing discharge locations for the properties to the south, and existing Seminole County utilities; the entirety of the improvements can not be captured in a manner that directs stormwater runoff to the proposed on-site ponds. The proposed design will capture the northern portions of the improvements to the extent possible by the roadway grading, however the southern portion of the road will continue to flow as in the existing condition to the elliptical pipe directed under US 17 and ultimately to Lake Monroe. The treatment volume provided in the ponds (which ultimately discharge to Lake Monroe) will include the required treatment volume for the school street improvements.

6. Secondary System design

The secondary system will meet the design criteria set forth in the FDOT Drainage Manual, Chapter 3.

The proposed development will utilize a curb and gutter closed system (piped inlets to the proposed stormwater ponds) as much as practical.

Closed flumes are being utilized on-site in some areas to eliminate sumped conditions within Pond 200 and along the south side to School Street to minimize impacts to existing utilities and reduce the directly connected impervious areas. The use of the closed flumes permits the use of a shallow swale between the back of curb and southern right of way line to ensure existing flow patterns are maintained.

E. School Street

The reconstruction limits of School Street will shift the alignment slightly north to avoid impacts to other properties and existing utilities. School Street has a posted speed limit of 25 MPH. School Street is a local road with a C3C context classification. Refer to Appendix H for the School Street Context Classification Review. The commitment to Seminole County for School Street between US 17 and just west of the parking facility entrance is for a 38' roadway section consisting of 2 – 12' lanes, 2 – 5' shoulders and Type F curb and gutter. Near the west edge of the parking lot entrance a traffic separator is required to discourage right turn movements from the truck parking. An 8' sidewalk is required along the northerly back of curb connecting to the US 17 sidewalk and terminating in the parking lot. Where not in conflict with the commitments made to Seminole County, the Florida Green Book, FGB, (Manual of Uniform Minimum Standards for Design Construction and Maintenance for Streets and Highways) design criteria for a local roadway with C3 context classification is used.

Design Speed: A 25-mph design speed is utilized for School Street is consistent with the subject property, truck parking, the fact that no other properties have direct access to School Street in the reconstruction limits of this project and commitment to install the Seminole County requested traffic separator just west of the truck parking entrance. The use of a 25-mph design speed is consistent with the FBG.

Table 3 – 1 Minimum and Maximum Design Speed (mph)

Facility ¹		AADT (vpd)	Terrain	Design Speed (mph)
Freeways	Rural	All	Level and Rolling	70
	Urban	All	Level and Rolling	50 – 70 ²
Arterials	Rural	All	Level	60 – 70
			Rolling	50 – 70
	Urban	All	All	30 – 60 ³
Collectors	Rural	≥ 400	Level	60 – 65 (50 mph min for AADT 400 to 2000)
			Rolling	50 – 65 (40 mph min for AADT 400 to 2000)
		< 400	Level	40 – 60
			Rolling	30 – 60
	Urban	All	All	30 – 50 ³
Local	Rural	≥ 400	Level	50 – 60
			Rolling	40 – 60
		< 400	Level	30 – 50
			Rolling	20 – 40
	Urban	All	All	20 – 30 ⁴

Footnotes:

1. Urban design speeds are applicable to streets and highways located within designated urban boundaries as well as those streets and highways outside designated urban boundaries yet within small communities or urban like developed areas. Rural design speeds are applicable to all other rural areas.
2. A design speed of 70 mph should be used for urban freeways when practical. Lower design speeds should only be used in highly developed areas with closely spaced interchanges. For these areas a minimum design speed of 60 mph is recommended unless it can be shown lower speeds will be consistent with driver expectancy.
3. Lower speeds apply to central business districts and in more developed areas while higher speeds are more applicable to outlying and developing areas.
4. Since the function of urban local streets is to provide access to adjacent property, all design elements should be consistent with the character of activity on and adjacent to the street, and should encourage speeds generally not exceeding 30 mph.

Stopping Site Distance: The minimum stopping sight distance for School Street is 155 feet.

Table 3 – 4 Minimum Stopping Sight Distance

Design Speed (mph)	Stopping Sight Distance (feet)						
	Level (≤ 2%)						
		Downgrades			Upgrades		
		3%	6%	9%	3%	6%	9%
20	115	116	120	126	109	107	104
25	155	158	165	173	147	143	140
30	200	205	215	227	200	184	179
35	250	257	271	287	237	229	222
40	305	315	333	354	289	278	269
45	360	378	400	427	344	331	320
50	425	446	474	507	405	388	375
55	495	520	553	593	469	450	433
60	570	598	638	686	538	515	495
65	645	682	728	785	612	584	561
70	730	771	825	891	690	658	631

Source: 2011 AASHTO Greenbook, Table 3-1 Stopping Sight Distance on Level Roadways and Table 3-2 Stopping Sight Distance on Grades.

Horizontal Alignment: The alignment of School Street within the limits of this project is a tangent section. The reconstruction limits of School Street will be a normal crown section with a cross slope of 0.015 until the transition point to tie to existing. The cross slope will transition to a continuous cross slope of 0.011+/- . The slope transition rate does not exceed 1:100. The horizontal alignment is shifted northward from the existing alignment to allow construction of the Seminole County required typical section and associated drainage structures within the existing right of way of School Street. The existing School Street right of way narrows considerably west of the intersection with US 17. The final configuration of the realignment of Orange Boulevard within the limits of the I-4 BtU3 project will need to be coordinated with the final alignment of School Street west of US 17.

Table 3 – 13 Superelevation Transition Slope Rates

Number of Lanes in One Direction	High Speed Roadways				Low Speed Roadways		
	Design Speed (mph)				Design Speed (mph)		
	25-40	45-50	55-60	65-70	25-35	40	45
1-Lane & 2-Lane	1:175	1:200	1:225	1:250	1:100	1:125	1:150
3-Lane	---	1:160	1:180	1:200			
4-Lane or more	---	1:150	1:170	1:190			

High Speed Roadways:

1. The length of superelevation transition is to be determined by the relative slope rate between the travel way edge of pavement and the profile grade, except that the minimum length of transition is 100 feet.
2. For additional information on transitions, see the [Standard Plans, Index 000-510](#).

Low Speed Roadways:

1. The length of superelevation transition is to be determined by the relative slope rate between the travel way edge of pavement and the profile grade, except that the minimum length of transition is 50 feet for design speeds 25-35 mph and 75 ft. for design speeds 40-45.
2. A slope rate of 1:125 may be used for 45 mph under restricted conditions.
3. For additional information on transitions, see [Standard Plans, Index 000-511](#).

The southside of School Street transitions to the existing alignment with

C.4.b Maximum Deflections in Alignment without Curves

The point where tangents intersect is known as the point of intersection (PI). Although the use of a PI with no horizontal curve is discouraged, there may be conditions where it is necessary. The maximum deflection criteria without a horizontal curve are as follows:

- Flush shoulder and curbed roadways with design speed 40 mph and less is 2° 00' 00".

**Table 3 – 12 Minimum Radii (feet) for Design Superelevation Rates
Low Speed Local Roads ($e_{max} = 0.05$)**

e - ft/ft	Design Speed (mph)							
	10	15	20	25	30	35	40	45
0.05	16	41	83	149	240	355	508	675
0.045	16	41	85	152	245	363	520	692
0.04	16	42	86	154	250	371	533	711
0.035	16	42	87	157	255	380	547	730
0.03	16	43	89	160	261	389	561	750
0.025	16	43	90	163	267	398	577	771
0.02	17	44	92	167	273	408	593	794
0.015	17	45	94	170	279	419	610	818
0.01	17	45	95	174	286	430	627	844
0.005	17	46	97	177	293	441	646	871
0	18	47	99	181	300	454	667	900
-0.01	18	48	103	189	316	480	711	964
-0.02	19	50	107	198	333	510	762	1038
-0.03 ¹	19	52	111	208	353	544	821	1125
-0.04 ¹	20	54	116	219	375	583	889	1227
-0.05 ¹	20	56	121	231	400	628	970	1350

1. Negative superelevation values beyond -0.02 feet per foot should be used only for unpaved surfaces such as gravel, crushed stone, and earth.

Vertical Alignment: The vertical alignment of School Street within this section is controlled by existing the vertical alignment of US 17 and the alignment of School Street west of the project limits. The criteria for the vertical alignment of School Street is the Florida Green Book for Local Road, C3 and design speed of 25-mph. The only land using having direct access to School Street within the limits of this project is the truck parking facility.

Table 3 – 16 Maximum Grades (in Percent)

Type of Roadway	Level Terrain											Rolling Terrain										
	Design Speed (mph)											Design Speed (mph)										
	20	25	30	35	40	45	50	55	60	65	70	20	25	30	35	40	45	50	55	60	65	70
Freeway ¹	---	---	---	---	---	---	4	4	3	3	3	---	---	---	---	---	5	5	4	4	4	4
Arterial	Rural											Urban										
	---	---	---	---	5	5	4	4	3	3	3	---	---	---	---	6	6	5	5	4	4	4
Collector ²	Rural											Urban										
	7	7	7	7	7	7	6	6	5	---	---	10	10	9	9	8	8	7	7	6	---	---
Local ³	Rural											Urban										
	8	8	8	8	8	8	7	7	6	---	---	12	12	11	10	10	9	8	8	7	---	---

Source: 2011 AASHTO Greenbook, Tables 5-2, 6-2, 6-8, 7-2, 7-4, 8-1.

Notes: 1. Grades 1% steeper than the value shown may be provided in urban areas with right of way constraints.

2. Short lengths of grade (< 500 feet in length), one-way downgrades, and grades on low volume collectors may be up to 2% steeper than the grades shown above.

3. Residential street grade should be as level as practical, consistent with surrounding terrain, and less than 15%. Streets in commercial or industrial areas should have grades less than 8%, and flatter grades should be encouraged.

Table 3 – 17 Maximum Change in Grade Without Using Vertical Curve

Design Speed (mph)	20	25	30	35	40	45	50	55	60	65	70
Maximum Change in Grade in Percent	1.20	1.10	1.00	0.90	0.80	0.70	0.60	0.50	0.40	0.30	0.20

Topic # 625-000-015
Manual of Uniform Minimum Standards
for Design, Construction and Maintenance
for Streets and Highways

2018

Table 3 – 18 Rounded K Values for Minimum Lengths Vertical Curves (Stopping Sight Distance)

(Based upon an eye height of 3.50 feet and an object height of 2 feet above the road surface)											
L = KA L = Length of Vertical Curve, A = Algebraic Difference of Grades in Percent											
Design Speed (mph)	20	25	30	35	40	45	50	55	60	65	70
K Values for Crest Vertical Curves	7	12	19	29	44	61	84	114	151	193	247
K Values for Sag Vertical Curves	17	26	37	49	64	79	96	115	136	157	181
<ul style="list-style-type: none"> The length of vertical curve must never be less than three times the design speed of the highway. Curve lengths computed from the formula $L = KA$ should be rounded upward when feasible. The minimum lengths of vertical curves to be used on collectors, arterials and freeways are shown in the table below: 											
Minimum Lengths for Vertical Curves on Collectors, Arterials, and Freeways (feet)											
Design Speed (mph)							50	60	70		
Crest Vertical Curves (feet)							300	400	500		
Sag Vertical Curves (feet)							200	300	400		

Design Documentation

As noted above, the project area for pond volume calculations includes that portion of School Street that is being reconstructed with Type F curb and gutter to facilitate site ingress and egress. However, due to the hydraulic gradients and connectivity requirements of off-site facilities, not all of the School Street modifications can be routed to this project's stormwater management system. For that part of the improved School Street that cannot be routed to the system, compensating storage will be provided by the continued inclusion of the area in the calculations. In addition, the School Street discharge under US 17 will be maintained and the post volume of

F. Utility Coordination

1. OneCall: A OneCall design ticket was submitted July 22, 2022.
2. AUN: Advanced Utility Notifications were submitted to the UAOs on July 21, 2022. 8

Refer to Appendix C for a copy of the 811 design ticket, the AUN and Utility Status Sheet

G. Utility Design

The utility provider for potable water, fire protection and sanitary sewer is Seminole County Environmental Services (SCES). Seminole County owns and maintains a 12" PVC water main along the southerly edge of pavement of Orange Boulevard, a 12" (material not known) water main along the westerly edge of pavement of US 17 and a 12" DIP water main along the southerly edge of pavement of School Street. SCES also has an out of service 12 AC (asbestos cement) water line in a 25' wide utility easement along the project's westerly right-of-way line, easterly right-of-way line of SR 400/I-4. SCES has requested that this easement remain active and the utility lines within the easement remain in place. Within the same 25' utility easement is a retired 10" AC force main. SCES has an 8" and a 10" force main along the southerly right of way of Orange Boulevard and south of the 12" pvc water main, an 8" (material not known) force main along the westerly right of way of US 17 and an 8" pvc force main along the northerly edge of pavement of School Street.

The current utility design proposes a force main connection to the existing force main in Orange Boulevard. The potable water line connection is proposed to tap the water main in School Street. The existing fire hydrants within School Street will be relocated behind the School Street sidewalk. Utility design and coordination is ongoing. Following the Phase I submittal VVH locations will be provided to the design team SUE firm ECHO for horizontal and vertical locates to determine and finalize the final utility routes. Utility demand calculations and utility sizes will be included in the Phase II submittal for early coordination with Seminole County Public Utilities immediately following the Phase II ERC resolution.

Power yards have been provided for future EV and Shore Power stations.

SECTION 3 Optional Pipe Material (*Pending Effort*)

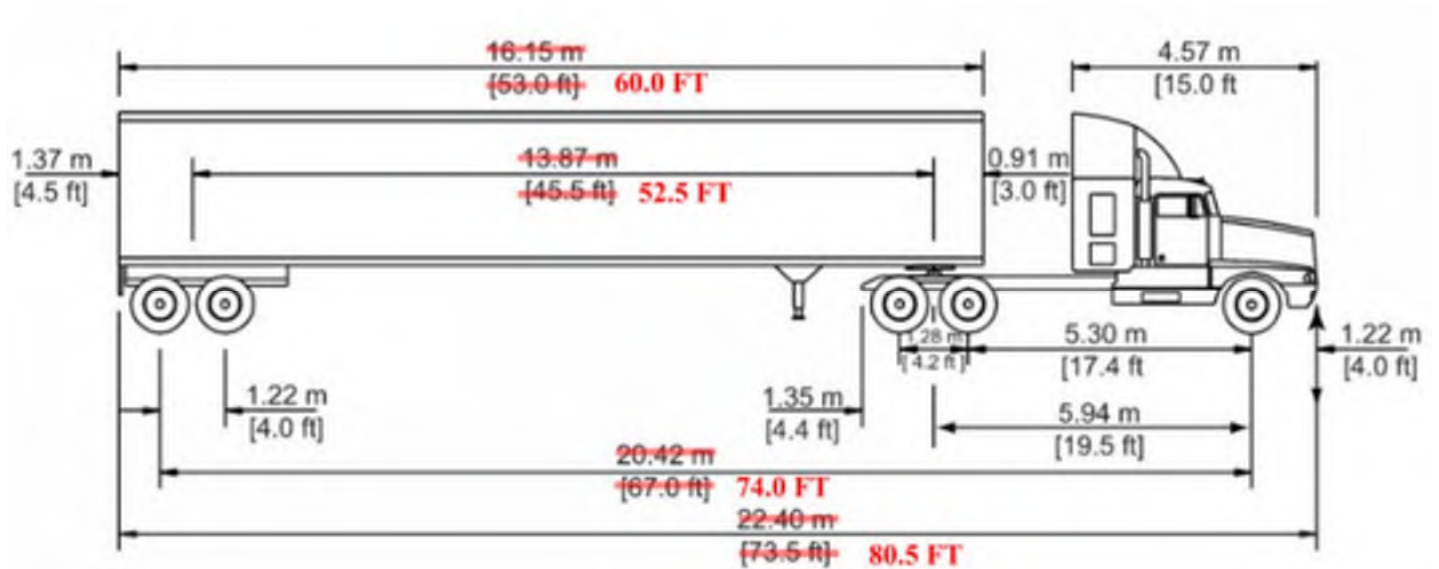
SECTION 4 BASE CLEARANCE ANALYSIS (PRELIMINARY PENDING PROJECT GEOTECHNICAL EXPLORATION)

Pending receipt of authorization to access the site and obtain the design borings, historical geotechnical reports for other projects over the subject site have been used to estimate a seasonal high ground water table (SHWT) elevation. The SHWT elevation is estimated to be elevation 7.5' NAVD 88. Refer to Appendix G for the tabulation of boring information and sketches.

SECTION 5 VE Coordination and Recommendations

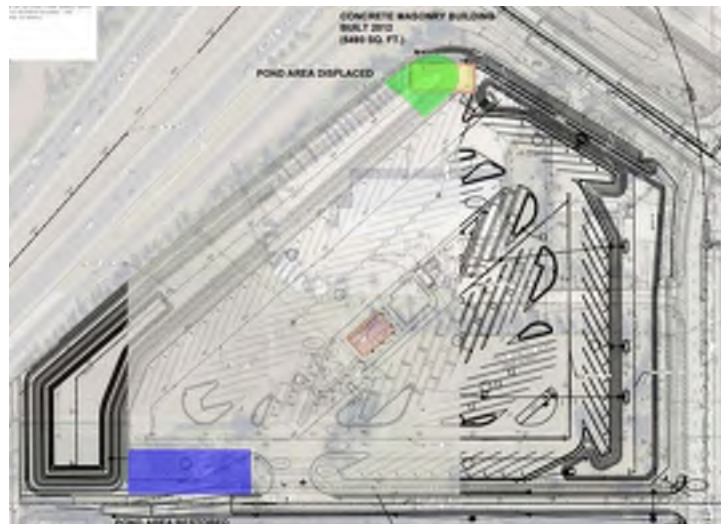
A Value Engineering analysis was performed for the PD&E project. As part of the VE effort the following recommendations were made with respect to the Seminole County site only, including “all site” comments, with the following discussions and/or responses.

Recommendation #18: Utilize a WB-67 Modified truck, as redlined below, as the control vehicle in lieu of the WB-109 to reduce pavement area:



Response: The WB-109 is not considered the controlling vehicle for this project. The design vehicles are the WB-62 FL and the WB-67D. The WB-109 was used in an AutoTurn evaluation to verify that should the Turnpike be closed with traffic routed to I-4, the WB-109 could access the site and navigate through it. Therefore, there was no design objection to using the WB-67 Mod as a control vehicle. The WB-67 Mod is accommodated within the site layout which is intended to accommodate the “average driver skill” for the above design vehicles. Therefore, this recommendation was **accepted by Bentley with no site changes made to “reduce” pavement footprint thereby restricting turning maneuvers.** The AutoTurn analysis for the WB-67 Mod is included here in.

Recommendation #19: Repurpose existing building.



Bentley recommends rejection of this recommendation based on the following disadvantages:

- Building was originally constructed in 2012 under obsolete building code and wind load criteria
- SCPA Data: Stone Veneer with wood or metal studs
- Building modification costs cannot be determined without an evaluation of the existing building and building systems
- Building system modifications anticipate
 - Openings
 - Roof
 - Plumbing
 - Slab
 - Electrical
 - HVAC
 - Exterior Finishes
- To determine the extent of building modifications needed, the original plans would need to be obtained along with any modifications from the owner. An existing field assessment of the building condition would also be required.
- The existing building would not be centrally located as requested by the trucking industry input.
- The existing building's finished floor elevation is approximately 12.4' NAVD 88, or approximately 1' below the top of proposed pond maintenance berm and at the current design site low point elevation resulting in the need for a pond covering a larger site area to provide the same treatment and attenuation volumes
- Redesign of the site would be required with a decrease in truck parking.

The final VE Resolution Memorandum is pending as of 7/19/2023.

754,425.89 SF, 17.32 AC
78,885.25 SF, 1.81 AC
833,311.14 SF, 19.13 AC



(PROCESSED ON REQUEST) MOLLY A (DEB)WIND P.E. LICENSE NUMBER: 64860 BENTLEY GROUP INC. 651 WEST WARREN AVENUE, SUITE 200 LONGWOOD, FLORIDA 32750	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		<i>EXISTING CONDITIONS</i>
	ROAD NO.	COUNTY	
	SR 400	SEMINOLE	
		FINANCIAL PROJECT ID	
		468445-1-52-01	

1. ONE NON-ACCESSIBLE EV & SP SPACE IS OVER SIZED.
2. ALL ACCESSIBLE TRUCK PARKING SPACES ARE OVER SIZED.
3. ALL OVER SIZED TRUCK PARKING SPACES ARE 15' WIDE WITH AN INTERLOCK DIMENSION GREATER THAN 80' FEET
4. ALL STANDARD TRUCK PARKING SPACES ARE 15' WIDE WITH AN INTERLOCK DIMENSION OF 80'.
5. AREA SHOWN HERE ON AS DELIVERIES IS FOR VENDING DELIVERIES AND OVERSIZE MAINTENANCE VEHICLES. THE AREA WILL BE SIGNED FOR DELIVERIES ONLY.
6. SIGNING AND MARKING SHOWN HERE ON IS PRELIMINARY.
7. PET WASTE BAG AND RECEPTACLE STATIONS WILL BE PROVIDED IN THE PET ENCLOSURE AND AT VARIOUS LOCATIONS THROUGHOUT THE SITE. LOCATIONS ARE TO BE DETERMINED.
8. THE INTENT OF THE LAYOUT SHOWN IS FOR TRUCK TO BACK IN PARK BASED ON THE DIRECTIONAL ARROWS SHOWN WITH THE EXCEPTION OF THE CENTRAL ISLAND SPACES EITHER SIDE OF THE RESTROOM BUILDING. THE ACCESSIBLE AND EV/SP SPACES IN THIS LOCATION ARE INTENDED TO BE PULL IN SPACES.

PARKING:	
ACCESSIBLE TRUCK PARKING	4 SPACES (3 EV/SP SPACES)
ACCESSIBLE TRUCK PARKING REQUIRED	4 SPACES
EV & SP TRUCK PARKING	20 EV/SP SPACES (15%)
STANDARD WB-62FL PARKING	108 SPACES
OVER SIZED TRUCK PARKING	4 SPACES
TOTAL TRUCK PARKING SPACES	133 SPACES
STAFF & VISITOR STD. PARKING	5 SPACES
STAFF & VISITOR ACCESSIBLE PARKING	1 SPACE
REQUIRED ACCESSIBLE PARKING	1 SPACES
TOTAL STAFF & VISITOR PARKING	6 SPACES

SITE AREA	754,425.89 SF, 17.32 AC
OFF SITE PROJECT AREA*	78,885.25 SF, 1.81 AC
TOTAL PROJECT AREA	833,311.14 SF, 19.13 AC
SITE IMPERVIOUS AREA	517423.83 SF, 11.88 AC
OFF SITE IMPERVIOUS AREA*	48,870.92 SF, 1.12 AC
TOTAL IMPERVIOUS AREA	566,294.75 SF, 13.00 AC

SITE PERVIOUS AREA	237,002.06 SF, 5.44 AC
OFF SITE PERVIOUS AREA*	30,014.33 SF, 0.69 AC
TOTAL PERVIOUS AREA	267,016.39 SF, 6.13 AC

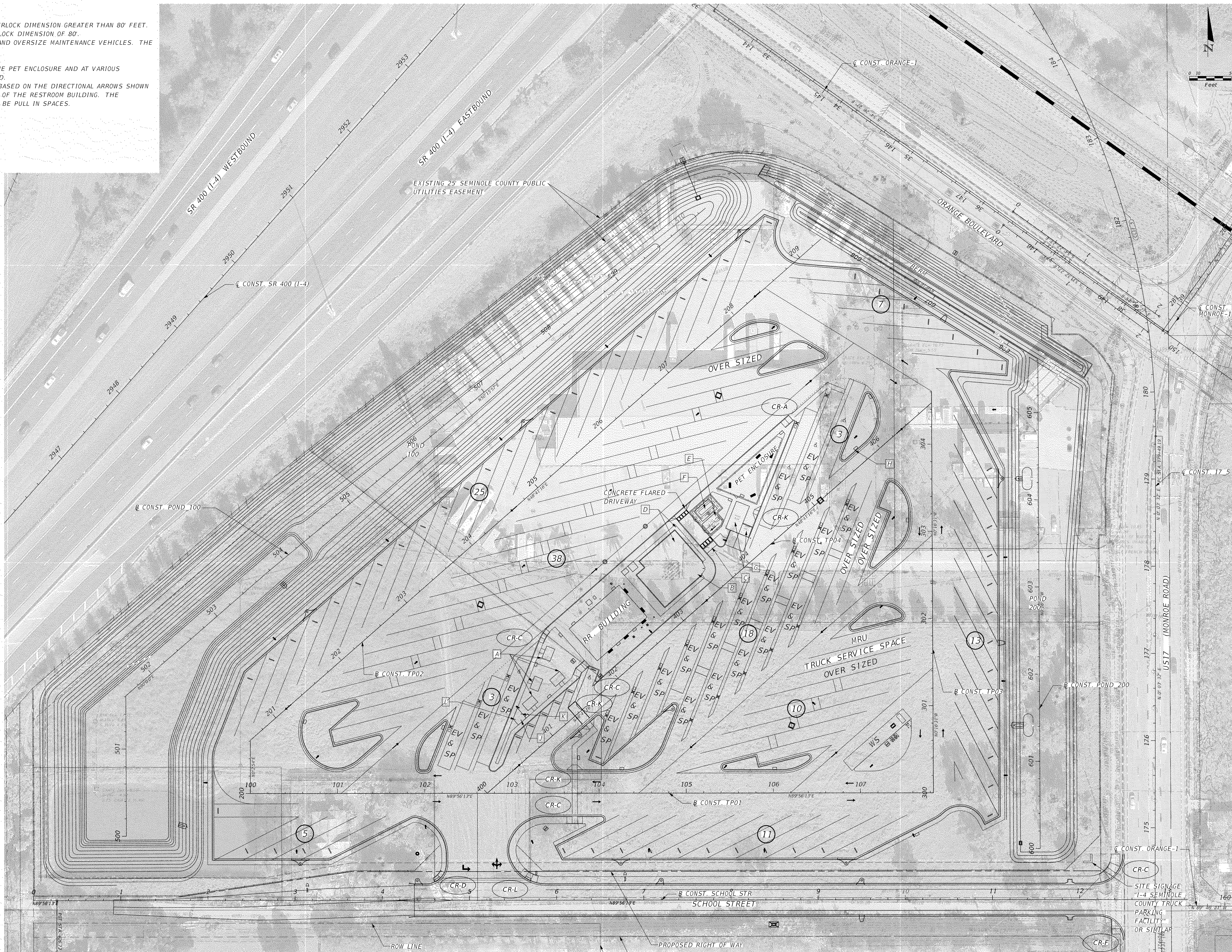
*INCLUDES PRELIMINARY ROW DEDICATION AREA TO SEMINOLE COUNTY FOR SCHOOL STREET WIDENING AND PEDESTRIAN IMPROVEMENTS

MOBILE UNIT1 SPACE
REPAIR VEHICLE SPACE1 SPACE

EV	ELECTRIC VEHICLE
MRU	MOBILE REPAIR UNIT
SP	SHORE POWER
WS	WINDSHIELD WASH

PARKING COUNT FOR A CONTIGUOUS GROUP OF SPACES

A	14' X 26' PAVILION
B	3,500 SF FOR FUTURE EV & SP POWER WALL WITH BLOCK WALL SCREEN ENCLOSURE
C	RAISED (6") EQUIPMENT DRIVE
D	TRANSFORMER PADS (1-225 KVA FOR RR BLDG AND 1- TBD FOR POWER YARD)
E	LIFT STATION
F	RESTROOM AND MAINTENANCE BUILDING BACKUP GENERATOR
G	MAINTENANCE BUILDING AND DUMPSTER ENCLOSURE
H	WASTE AND RECYCLE RECEPTACLES CONCRETE SLAB (4.5' X 2.5') TYPICAL OF 22 AS SHOWN
I	6' LONG BENCHES UNDER RESTROOM BUILDING CANOPY, TYPICAL OF 5 AS SHOWN
J	25' FLAG POLE (US FLAG)
K	20' FLAG POLE (POW FLAG)
L	20' FLAG POLE (STATE FLAG)



PRELIMINARY

ENGINEER OF RECORD		STATE OF FLORIDA		
MOLLY A. DEVIVERO LICENSE NUMBER: 64860 BENTLEY GROUP INC. 651 WEST WARREN AVENUE, SUITE 200 LONGWOOD, FLORIDA 32750		DEPARTMENT OF TRANSPORTATION		
		ROAD NO.	COUNTY	FINANCIAL PROJECT ID
		SR 400	SEMINOLE	446445-1-52-01

ROADWAY PLAN

... SHEET NO.	11
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[illegible]

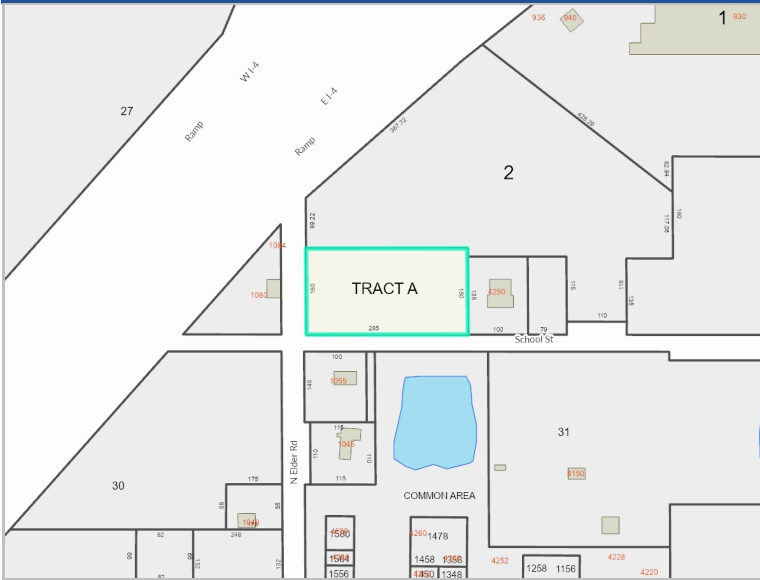
Property Record Card



Parcel 21-19-30-511-0A00-0000

Property Address ,

Parcel Location



Site View

Sorry, No Image
Available at this Time

Parcel Information

Parcel	21-19-30-511-0A00-0000
Owner(s)	LAKE MONROE I-4 CENTER PROPERTY OWNERS ASSN INC
Property Address	,
Mailing	C/O SPT PO BOX 84000 SAN DIEGO, CA 92138-4000
Subdivision Name	LAKE MONROE I-4 CENTER
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	1020-VAC COMM RETENTION/CONSERVATION/ROADS/COMMON AREA
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	0	0
Depreciated Bldg Value		
Depreciated EXFT Value		
Land Value (Market)		
Land Value Ag		
Just/Market Value	\$0	\$0
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$0	\$0

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$0.00
2023 Tax Bill Amount	\$0.00

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

TRACT A LAKE MONROE I-4 CENTER PLAT BOOK {82} PAGES {47-50}

Taxes			
Taxing Authority	Assessment Value	Exempt Values	Taxable Value
ROAD DISTRICT	\$0	\$0	\$0
SJWM(Saint Johns Water Management)	\$0	\$0	\$0
FIRE	\$0	\$0	\$0
COUNTY GENERAL FUND	\$0	\$0	\$0
Schools	\$0	\$0	\$0

Sales						
Description	Date	Book	Page	Amount	Qualified	Vac/Imp

Land					
Method	Frontage	Depth	Units	Units Price	Land Value

Building Information					
Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date

Extra Features				
Description	Year Built	Units	Value	New Cost

Zoning			
Zoning	Zoning Description	Future Land Use	Future Land Use Description
PD	Higher Intensity Planned Development – Target Industry	HIPTI	Planned Development

Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA

Political Representation				
Commissioner	US Congress	State House	State Senate	Voting Precinct
Dist 5 - Andria Herr	Dist 7 - Cory Mills	Dist 36 - RACHEL PLAKON	Dist 10 - Jason Brodeur	6

School Information		
Elementary School District	Middle School District	High School District
Region 1	Markham Woods	Seminole

Property Record Card



Parcel 21-19-30-511-0000-0020

Property Address ,

Parcel Location



Site View

Sorry, No Image
Available at this Time

Parcel Information

Parcel	21-19-30-511-0000-0020
Owner(s)	DAN REALTY GROUP LLC
Property Address	,
Mailing	450 S RONALD REAGAN BLVD LONGWOOD, FL 32750-5405
Subdivision Name	LAKE MONROE I-4 CENTER
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	1013-VAC COMM W/ SITE IMPROVEMENTS
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	0	0
Depreciated Bldg Value		
Depreciated EXFT Value	\$15,821	\$14,525
Land Value (Market)	\$1,061,397	\$1,061,397
Land Value Ag		
Just/Market Value	\$1,077,218	\$1,075,922
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$1,077,218	\$1,075,922

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$14,318.37
2023 Tax Bill Amount	\$14,318.37

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

LOT 2 LAKE MONROE I-4 CENTER PLAT BOOK {82} PAGES {47-50}

Taxes

Taxing Authority	Assessment Value	Exempt Values	Taxable Value
ROAD DISTRICT	\$1,077,218	\$0	\$1,077,218
SJWM(Saint Johns Water Management)	\$1,077,218	\$0	\$1,077,218
FIRE	\$1,077,218	\$0	\$1,077,218
COUNTY GENERAL FUND	\$1,077,218	\$0	\$1,077,218
Schools	\$1,077,218	\$0	\$1,077,218

Sales

Description	Date	Book	Page	Amount	Qualified	Vac/Imp
SPECIAL WARRANTY DEED	05/16/2019	09357	1586	\$2,399,000	Yes	Improved

Land

Method	Frontage	Depth	Units	Units Price	Land Value
ACREAGE			0.5	\$535.00	\$268
SQUARE FEET			168701	\$6.29	\$1,061,129

Building Information Permits

Permit #	Description	Agency	Amount	CO Date	Permit Date
----------	-------------	--------	--------	---------	-------------

Extra Features

Description	Year Built	Units	Value	New Cost
6' CHAIN LINK FENCE	01/01/2012	2,256	\$15,821	\$26,350

Zoning

Zoning	Zoning Description	Future Land Use	Future Land Use Description
PD	Higher Intensity Planned Development – Target Industry	HIPTI	Planned Development

Utility Information

Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA

Political Representation

Commissioner	US Congress	State House	State Senate	Voting Precinct
Dist 5 - Andria Herr	Dist 7 - Cory Mills	Dist 36 - RACHEL PLAKON	Dist 10 - Jason Brodeur	6

School Information

Elementary School District	Middle School District	High School District
Region 1	Markham Woods	Seminole

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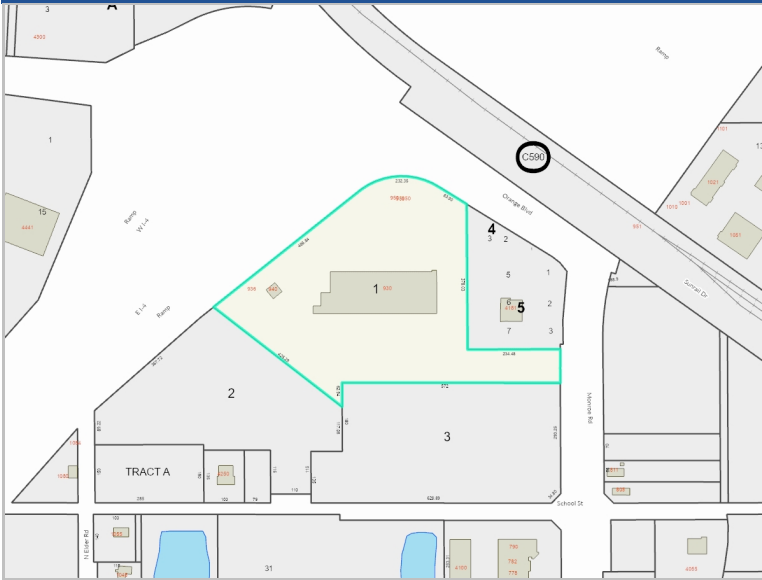
Property Record Card



Parcel 21-19-30-511-0000-0010

Property Address 950 MONROE RD SANFORD, FL 32771

Parcel Location



Site View



21193051100000010 05/03/2023

Parcel Information

Parcel	21-19-30-511-0000-0010
Owner(s)	DONNIE MYERS HOLDINGS LLC
Property Address	950 MONROE RD SANFORD, FL 32771
Mailing	930 MONROE RD SANFORD, FL 32771-8823
Subdivision Name	LAKE MONROE I-4 CENTER
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	2704-MISC MOTOR SALES
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	3	3
Depreciated Bldg Value	\$1,928,010	\$1,844,973
Depreciated EXFT Value	\$458,070	\$414,887
Land Value (Market)	\$1,482,800	\$1,482,800
Land Value Ag		
Just/Market Value	\$3,868,880	\$3,742,660
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$3,868,880	\$3,742,660

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$49,807.32
2023 Tax Bill Amount	\$49,807.32

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

LOT 1 LAKE MONROE I-4 CENTER PLAT BOOK {82} PAGES {47-50}

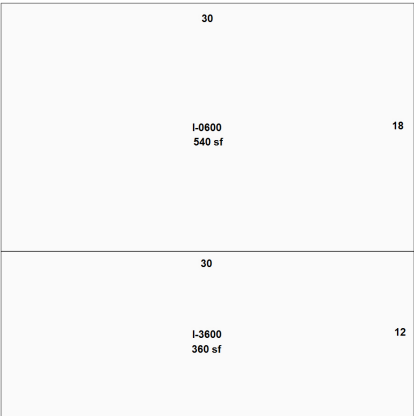
ROAD DISTRICT	\$3,868,880	\$0	\$3,868,880
FIRE	\$3,868,880	\$0	\$3,868,880
Schools	\$3,868,880	\$0	\$3,868,880

Land					
Method	Frontage	Depth	Units	Units Price	Land Value
SQUARE FEET			235660	\$6.29	\$1,482,301
ACREAGE			0.998	\$500.00	\$499

Building Information									
#	Description	Year Built Actual/Effective	Stories	Total SF	Ext Wall	Adj Value	Repl Value	Appendages	
1	MASONRY PILASTER .	2012	1	5490.00	STONE VENEER WITH WOOD OR METAL STUDS	\$592,800	\$687,304	Description	Area
								OPEN PORCH FINISHED	210.00
								OPEN PORCH FINISHED	44.00
								OPEN PORCH FINISHED	44.00

Building 1 - Page 1

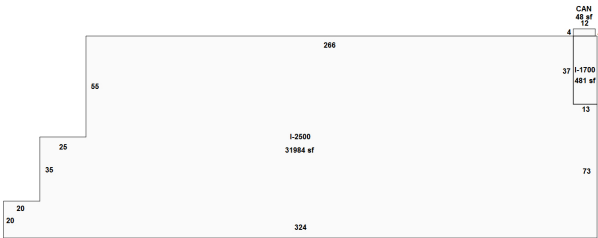
#	Description	Year Built Actual/Effective	Stories	Total SF	Ext Wall	Adj Value	Repl Value	Appendages	
2	MASONRY PILASTER .	2012	1	900.00	CONCRETE BLOCK-STUCCO - MASONRY	\$75,153	\$87,134	Description	Area



Drawn by: Ryan Stueck

Building 2 - Page 1

#	Description	Year Built Actual/Effective	Stories	Total SF	Ext Wall	Adj Value	Repl Value	Appendages	
3	STEEL/PRE ENGINEERED.	2012	1	32465.00	METAL PREFINISHED - INSULATED	\$1,260,057	\$1,500,068	Description	Area
								CANOPY	48.00



Drawn by: Ryan Stueck

Building 3 - Page 1

Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date

Extra Features				
Description	Year Built	Units	Value	New Cost
COMMERCIAL CONCRETE DR 4 IN	01/01/2013	135,584	\$458,070	\$631,821

Zoning								
Zoning		Zoning Description		Future Land Use		Future Land Use Description		
PD		Higher Intensity Planned Development – Target Industry		HIPTI		Planned Development		
Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA
Political Representation								
Commissioner		US Congress		State House		State Senate		Voting Precinct
Dist 5 - Andria Herr		Dist 7 - Cory Mills		Dist 36 - RACHEL PLAKON		Dist 10 - Jason Brodeur		6
School Information								
Elementary School District			Middle School District			High School District		
Region 1			Markham Woods			Seminole		
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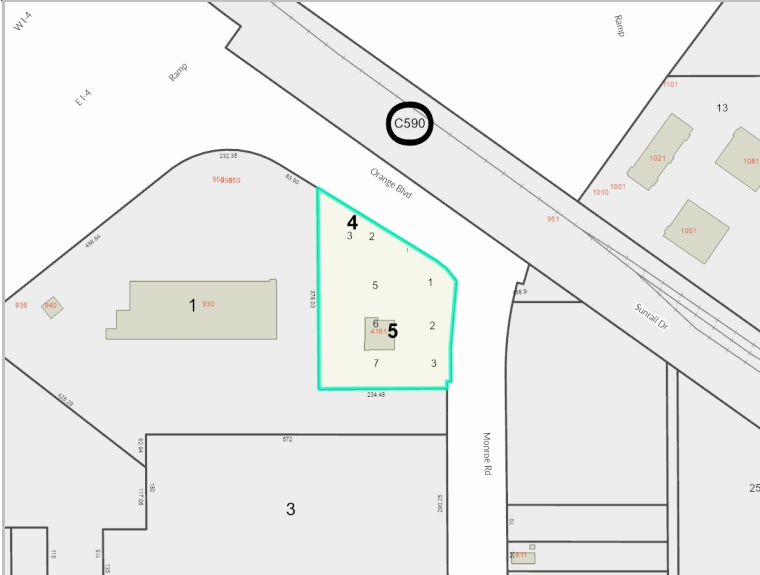
Property Record Card



Parcel 21-19-30-502-0400-0000

Property Address 4181 ORANGE BLVD SANFORD, FL 32771

Parcel Location



Site View



21193050204000000 05/03/2023

Parcel Information

Parcel	21-19-30-502-0400-0000
Owner(s)	CIRCLE K STORES INC
Property Address	4181 ORANGE BLVD SANFORD, FL 32771
Mailing	Attn: REAL ESTATE DEPT PO BOX 52085 DC-17 PHOENIX, AZ 85072-2085
Subdivision Name	MONROE TOWN OF
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	1104-CONVENIENCE STORE WITH GAS
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	1	1
Depreciated Bldg Value	\$136,400	\$137,507
Depreciated EXFT Value	\$80,283	\$71,966
Land Value (Market)	\$733,165	\$733,165
Land Value Ag		
Just/Market Value	\$949,848	\$942,638
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$949,848	\$942,638

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$12,544.63
2023 Tax Bill Amount	\$12,544.63

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

PT OF BLKS 4 & 5 & VACD STS DESC AS BEG 18 FT W + 164.58 FT N OF INT OF W R/W MONROE RD + C/L OF VACD ST BET BLKS 5 + 8 RUN S 87.87 FT W 286.87 FT N TO SLY R/W ORANGE BLVD SELY ON R/W TO A PT N OF BEG S TO BEG (LESS RD)
TOWN OF MONROE
PB 1 PG 97

ROAD DISTRICT	\$949,848	\$0	\$949,848
FIRE	\$949,848	\$0	\$949,848
Schools	\$949,848	\$0	\$949,848

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WARRANTY DEED	08/01/1988	01982	0569	\$100	No	Improved
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WARRANTY DEED	04/01/1983	01455	0727	\$150,000	No	Improved
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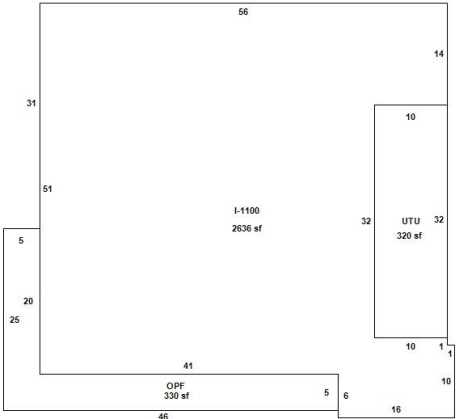
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SQUARE FEET	62291	\$11.77	\$733,165
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Building Information

#	Description	Year Built Actual/Effective	Stories	Total SF	Ext Wall	Adj Value	Repl Value	Appendages
1	MASONRY PILASTER .	1990	1	2636.00	FACE BLOCK - MASONRY	\$136,400	\$215,652	

Description	Area
UTILITY UNFINISHED	320.00
OPEN PORCH FINISHED	330.00



Building 1 - Page 1

Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date
04171	INSTALL UNDERGROUND GAS TANKS	County	\$2,000		4/1/2002
06068	INTERIOR - KANGAROO	County	\$70,000	8/2/2005	3/23/2005
00232	SIGN	County	\$0		1/8/2004
09807	SIGN BOX	County	\$9,000		12/22/2010
06871	UNDERGROUND GAS TANKS	County	\$152,000		6/27/2008
07045	ELECTRICAL	County	\$1,990		9/7/2010
00537	REROOF	County	\$16,500		1/23/2009
05341	GENERAL REPAIR; HANDY WAY	County	\$11,000		7/1/1998
13437	SIGN	County	\$8,500		11/15/2018
05248	HANDYWAY FOOD STORE EXAUST HD	County	\$2,100		8/1/1996
21519	4181 ORANGE BLVD: SIGN (POLE,WALL,FACIA)-signage [MONROE TOWN OF]	County	\$1,200		12/6/2021

Extra Features				
Description	Year Built	Units	Value	New Cost
POLE LIGHT 2 ARM	06/01/1990	2	\$7,210	\$7,210
CANOPY GOOD COMM	10/01/1990	1,800	\$10,879	\$27,198
WALKS CONC COMM	10/01/1990	1,500	\$2,796	\$6,990
COMMERCIAL CONCRETE DR 4 IN	10/01/1990	8,515	\$15,872	\$39,680
BLOCK WALL	10/01/1990	312	\$1,148	\$2,870
COMMERCIAL ASPHALT DR 2 IN	10/01/1990	30,000	\$29,400	\$73,500
POLE LIGHT 1 ARM	06/01/1990	7	\$12,978	\$12,978

Zoning			
Zoning	Zoning Description	Future Land Use	Future Land Use Description
PD	Higher Intensity Planned Development – Target Industry	HIPTI	Planned Development

Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA

Political Representation				
Commissioner	US Congress	State House	State Senate	Voting Precinct
Dist 5 - Andria Herr	Dist 7 - Cory Mills	Dist 36 - RACHEL PLAKON	Dist 10 - Jason Brodeur	6

School Information		
Elementary School District	Middle School District	High School District
Region 1	Markham Woods	Seminole

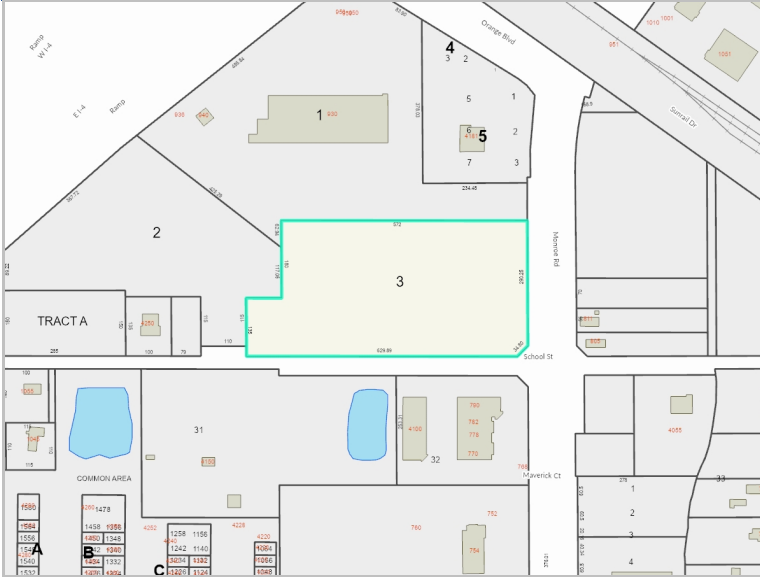
Property Record Card



Parcel 21-19-30-511-0000-0030

Property Address ,

Parcel Location



Site View

Sorry, No Image
Available at this Time

Parcel Information

Parcel	21-19-30-511-0000-0030
Owner(s)	DAN REALTY GROUP LLC
Property Address	,
Mailing	450 S RONALD REAGAN BLVD LONGWOOD, FL 32750-5405
Subdivision Name	LAKE MONROE I-4 CENTER
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	10-VAC GENERAL-COMMERCIAL
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	0	0
Depreciated Bldg Value		
Depreciated EXFT Value		
Land Value (Market)	\$1,195,974	\$1,195,974
Land Value Ag		
Just/Market Value	\$1,195,974	\$1,195,974
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$1,195,974	\$1,195,974

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$15,916.02
2023 Tax Bill Amount	\$15,916.02

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

LOT 3 LAKE MONROE I-4 CENTER PLAT BOOK {82} PAGES {47-50}

Taxes			
Taxing Authority	Assessment Value	Exempt Values	Taxable Value
ROAD DISTRICT	\$1,195,974	\$0	\$1,195,974
SJWM(Saint Johns Water Management)	\$1,195,974	\$0	\$1,195,974
FIRE	\$1,195,974	\$0	\$1,195,974
COUNTY GENERAL FUND	\$1,195,974	\$0	\$1,195,974
Schools	\$1,195,974	\$0	\$1,195,974

Sales						
Description	Date	Book	Page	Amount	Qualified	Vac/Imp
SPECIAL WARRANTY DEED	05/16/2019	09357	1586	\$2,399,000	Yes	Vacant

Land					
Method	Frontage	Depth	Units	Units Price	Land Value
SQUARE FEET			190139	\$6.29	\$1,195,974

Building Information					
Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date

Extra Features				
Description	Year Built	Units	Value	New Cost

Zoning			
Zoning	Zoning Description	Future Land Use	Future Land Use Description
PD	Higher Intensity Planned Development – Target Industry	HIPTI	Planned Development

Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA

Political Representation				
Commissioner	US Congress	State House	State Senate	Voting Precinct
Dist 5 - Andria Herr	Dist 7 - Cory Mills	Dist 36 - RACHEL PLAKON	Dist 10 - Jason Brodeur	6

School Information		
Elementary School District	Middle School District	High School District
Region 1	Markham Woods	Seminole

Property Record Card



Parcel 16-19-30-5AC-0000-026B

Property Address SANFORD, FL 32771

Parcel Location



Site View

Sorry, No Image
Available at this Time

Parcel Information

Parcel	16-19-30-5AC-0000-026B
Owner(s)	NORTH RIVER FISHERIES INC
Property Address	SANFORD, FL 32771
Mailing	PO BOX 470002 LAKE MONROE, FL 32747-0002
Subdivision Name	ST JOSEPHS
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	00-VACANT RESIDENTIAL
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	0	0
Depreciated Bldg Value		
Depreciated EXFT Value		
Land Value (Market)	\$24,346	\$24,346
Land Value Ag		
Just/Market Value	\$24,346	\$24,346
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$0
P&G Adj	\$0	\$0
Assessed Value	\$24,346	\$24,346

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$324.00
2023 Tax Bill Amount	\$324.00

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

S 135 FT OF W 79 FT OF E
249 FT OF LOT 26
ST JOSEPHS
PB 1 PG 114

Taxes			
Taxing Authority	Assessment Value	Exempt Values	Taxable Value
ROAD DISTRICT	\$24,346	\$0	\$24,346
SJWM(Saint Johns Water Management)	\$24,346	\$0	\$24,346
FIRE	\$24,346	\$0	\$24,346
COUNTY GENERAL FUND	\$24,346	\$0	\$24,346
Schools	\$24,346	\$0	\$24,346

Sales						
Description	Date	Book	Page	Amount	Qualified	Vac/Imp
QUIT CLAIM DEED	06/01/2011	07594	1288	\$100	No	Vacant
QUIT CLAIM DEED	06/01/2011	07594	1290	\$100	No	Vacant
QUIT CLAIM DEED	06/01/2011	07594	1287	\$100	No	Vacant
WARRANTY DEED	06/01/2011	07594	1291	\$24,400	No	Vacant
QUIT CLAIM DEED	06/01/2011	07594	1289	\$100	No	Vacant
WARRANTY DEED	12/01/1979	01256	1723	\$5,500	Yes	Vacant
ARTICLES OF AGREEMENT	01/01/1974	01008	1545	\$5,000	No	Vacant

Land					
Method	Frontage	Depth	Units	Units Price	Land Value
ACREAGE			0.243	\$100,188.00	\$24,346

Building Information					
Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date
11285	DEMOLISH	County	\$3,140		12/1/2000

Extra Features				
Description	Year Built	Units	Value	New Cost

Zoning			
Zoning	Zoning Description	Future Land Use	Future Land Use Description
A-1	Higher Intensity Planned Development – Target Industry	HIPTI	Agricultural-1Ac

Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	NA	NA	NA	NA

Political Representation				
Commissioner	US Congress	State House	State Senate	Voting Precinct
Dist 5 - Andria Herr	Dist 7 - Cory Mills	Dist 36 - RACHEL PLAKON	Dist 10 - Jason Brodeur	6

School Information		
Elementary School District	Middle School District	High School District
Region 1	Markham Woods	Seminole

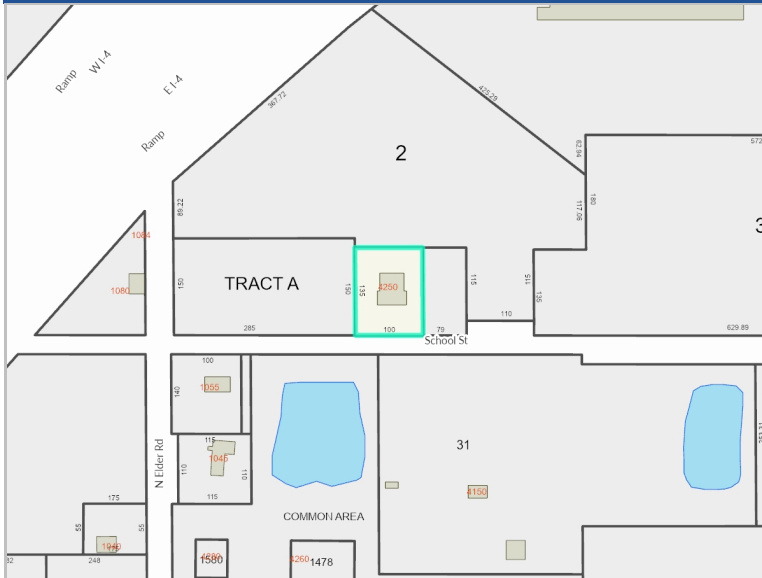
Property Record Card



Parcel 16-19-30-5AC-0000-026E

Property Address 4250 SCHOOL ST SANFORD, FL 32771

Parcel Location



Site View



1619305AC0000026E 05/04/2023

Parcel Information

Parcel	16-19-30-5AC-0000-026E
Owner(s)	NORTH RIVER FISHERIES INC
Property Address	4250 SCHOOL ST SANFORD, FL 32771
Mailing	PO BOX 470002 LAKE MONROE, FL 32747-0002
Subdivision Name	ST JOSEPHS
Tax District	01-COUNTY-TX DIST 1
DOR Use Code	01-SINGLE FAMILY
Exemptions	None
AG Classification	No

Value Summary

	2024 Working Values	2023 Certified Values
Valuation Method	Cost/Market	Cost/Market
Number of Buildings	1	1
Depreciated Bldg Value	\$42,577	\$41,710
Depreciated EXFT Value		
Land Value (Market)	\$24,846	\$24,846
Land Value Ag		
Just/Market Value	\$67,423	\$66,556
Portability Adj		
Save Our Homes Adj	\$0	\$0
Amendment 1 Adj	\$0	\$2,866
P&G Adj	\$0	\$0
Assessed Value	\$67,423	\$63,690

2023 Certified Tax Summary

2023 Tax Amount without Exemptions	\$885.73	2023 Tax Savings with Exemptions	\$22.73
2023 Tax Bill Amount	\$863.00		

* Does NOT INCLUDE Non Ad Valorem Assessments

Legal Description

W 100 FT OF E 349 FT OF S
135 FT OF LOT 26
ST JOSEPHS
PB 1 PG 114

Taxes			
Taxing Authority	Assessment Value	Exempt Values	Taxable Value
ROAD DISTRICT	\$67,423	\$0	\$67,423
SJWM(Saint Johns Water Management)	\$67,423	\$0	\$67,423
FIRE	\$67,423	\$0	\$67,423
COUNTY GENERAL FUND	\$67,423	\$0	\$67,423
Schools	\$67,423	\$0	\$67,423

Sales						
Description	Date	Book	Page	Amount	Qualified	Vac/Imp
WARRANTY DEED	06/01/1985	01648	1400	\$22,500	Yes	Improved
QUIT CLAIM DEED	12/01/1982	01425	1573	\$100	No	Improved
WARRANTY DEED	06/01/1980	01286	0300	\$13,500	Yes	Improved
WARRANTY DEED	03/01/1980	01269	1713	\$7,000	Yes	Improved

Land					
Method	Frontage	Depth	Units	Units Price	Land Value
ACREAGE			0.31	\$100,188.00	\$24,846

Building Information												
#	Description	Year Built**	Bed	Bath	Fixtures	Base Area	Total SF	Living SF	Ext Wall	Adj Value	Repl Value	Appendages
1	SINGLE FAMILY	1930	3	1.0	3	1,638	2,076	1,638	SIDING GRADE	\$42,577	\$106,442	<div> <div>Description</div> <div>Area</div> <div>OPEN PORCH UNFINISHED</div> <div>70.00</div> <div>OPEN PORCH FINISHED</div> <div>368.00</div> </div>
<div> </div>												

** Year Built (Actual / Effective)

Permits					
Permit #	Description	Agency	Amount	CO Date	Permit Date

Extra Features				
Description	Year Built	Units	Value	New Cost

Zoning								
Zoning		Zoning Description		Future Land Use		Future Land Use Description		
A-1		Higher Intensity Planned Development – Target Industry		HIPTI		Agricultural-1Ac		
Utility Information								
Fire Station	Power	Phone(Analog)	Water Provider	Sewer Provider	Garbage Pickup	Recycle	Yard Waste	Hauler
34.00	FPL	AT&T	SEMINOLE COUNTY UTILITIES	SEMINOLE COUNTY UTILITIES	MON/THU	THU	WED	Waste Pro
Political Representation								
Commissioner		US Congress		State House		State Senate		Voting Precinct
Dist 5 - Andria Herr		Dist 7 - Cory Mills		Dist 36 - RACHEL PLAKON		Dist 10 - Jason Brodeur		6
School Information								
Elementary School District			Middle School District			High School District		
Region 1			Markham Woods			Seminole		
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**Seminole County Government
Development Services Department
Planning and Development Division
Credit Card Payment Receipt**

If you have questions about your application or payment, please email us eplandesk@seminolecountyfl.gov or call us at: (407) 665-7371.

Receipt Details

Date: 10/10/2023 10:24:32 AM
Project: 23-80000143
Credit Card Number: 42*****3112
Authorization Number: 02962G
Transaction Number: 101023O13-992C7938-EA73-4F0A-9837-E620F3E6F082
Total Fees Paid: 52.50

Fees Paid

Description	Amount
CC CONVENIENCE FEE -- PZ	2.50
PRE APPLICATION	50.00
Total Amount	52.50